

TRACTATENBLAD

VAN HET

KONINKRIJK DER NEDERLANDEN

JAARGANG 2017 Nr. 18

A. TITEL

*Douaneovereenkomst inzake het internationale vervoer van goederen onder dekking van carnets TIR (TIR-Overeenkomst) (met Bijlagen);
Genève, 14 november 1975*

Voor een overzicht van de verdragsgegevens, zie verdragsnummers 002114 en 013440 in de Verdragenbank.

B. TEKST

Wijzigingen van 11 februari 2016

In overeenstemming met artikel 59, eerste en tweede lid, van de Overeenkomst heeft de Commissie van Beheer op zijn drieënzestigste zitting gehouden te Genève op 11 februari 2016 wijzigingen van de Bijlagen 2, 6 en 7 bij de Overeenkomst aangenomen.¹⁾

De Engelse tekst²⁾ van deze wijzigingen luidt als volgt:

Annex 2, Article 4, paragraph 2, (i)

For the existing text substitute

- (i) The sliding sheets, floor, doors and all other constituent parts of the load compartment shall be assembled either by means of devices which cannot be removed and replaced from the outside without leaving obvious traces, or by such methods as will produce a structure which cannot be modified without leaving obvious traces.

Annex 2, Article 4, paragraph 2, (iii)

For the existing text substitute

- (iii) The sliding sheet guidance, sliding sheet tension devices and other movable parts shall be assembled in such a way that when closed, and Customs sealed, doors and other movable parts cannot be opened or closed from the outside without leaving obvious traces. The sliding sheet guidance, sliding sheet tension devices and other movable parts shall be assembled in such a way that it is impossible to gain access to the load compartment without leaving obvious traces once the closing devices have been secured. An example of such a system of construction is given in sketch No. 9 appended to these Regulations.

¹⁾ Aangezien het voor wat betreft Bijlage 6 wijzigingen van de Explanatory Notes betreft, liggen deze ter inzage bij de Directie Douane en Verbruiksbelastingen van het Ministerie van Financiën en bij de Afdeling Verdragen van het Ministerie van Buitenlandse Zaken. Tevens zijn de Engelse, de Franse en de Russische tekst van de wijzigingen en de corrigenda (documenten ECE/TRANS/WP.30/AC.2/2016/10, ECE/TRANS/WP.30/AC.2/2016/10/Corr.1 (alleen Russisch) en ECE/TRANS/WP.30/AC.2/2016/10/Corr.2 (alleen Frans)) te raadplegen op http://www.unece.org/trans/bcf/ac2/ac2-off-documents_2016.html

²⁾ De Franse en de Russische tekst zijn niet opgenomen.

Annex 2, new Article 5

After the modified Article 4 insert

Article 5

Vehicles with a sheeted sliding roof

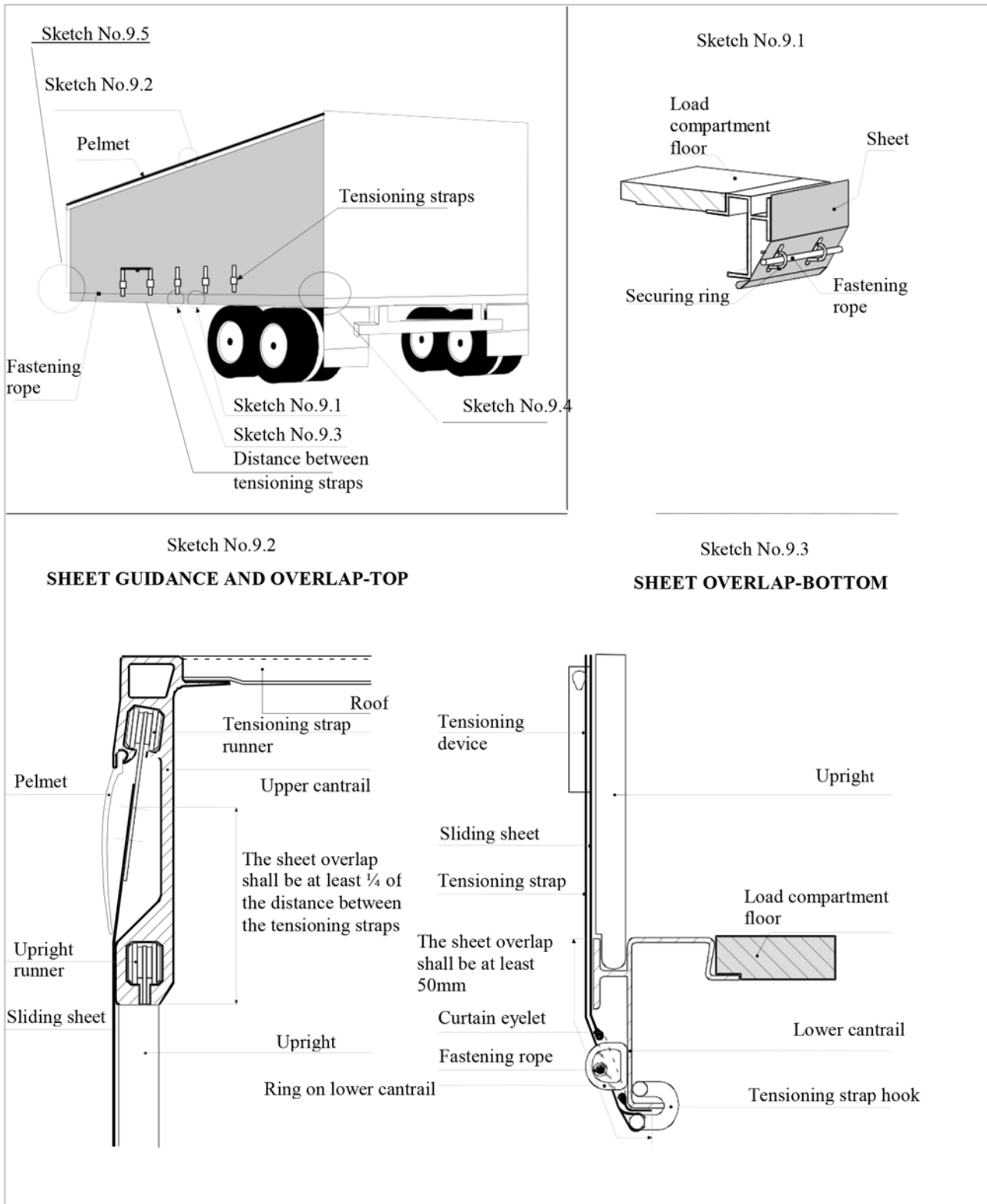
1. Where applicable, the provisions of Articles 1, 2, 3 and 4 of these Regulations shall apply to vehicles with a sheeted sliding roof. In addition, these vehicles shall conform to the provisions of this Article.
2. The sheeted sliding roof shall fulfil the requirements set out in (i) to (iii) below.
 - (i) The sheeted sliding roof shall be assembled either by means of devices which cannot be removed and replaced from the outside without leaving obvious traces, or by such methods as will produce a structure which cannot be modified without leaving obvious traces.
 - (ii) The sliding roof sheet shall overlap with the solid part of the roof at the front side of the load compartment, so that the roof sheet cannot be pulled over the top edge of the upper cantrail. In the length of the load compartment, at both sides, in the hem of the roof sheet, a pre-stressed steel cable shall be inserted in such a way that it cannot be removed and re-inserted without leaving obvious traces. The roof sheet shall be secured to the sliding carriage in such a way that it cannot be removed and re-secured without leaving obvious traces.
 - (iii) The sliding roof guidance, the sliding roof tension devices and other movable parts shall be assembled in such a way that when closed, and Customs sealed, doors, roof and other movable parts cannot be opened or closed from the outside without leaving obvious traces. The sliding roof guidance, sliding roof tension devices and other movable parts shall be assembled in such a way that it is impossible to gain access to the load compartment without leaving obvious traces once the closing devices have been secured.

An example of a possible system of construction is shown in sketch No.10, appended to these Regulations.

For the existing Sketch No. 9 substitute

Sketch No. 9

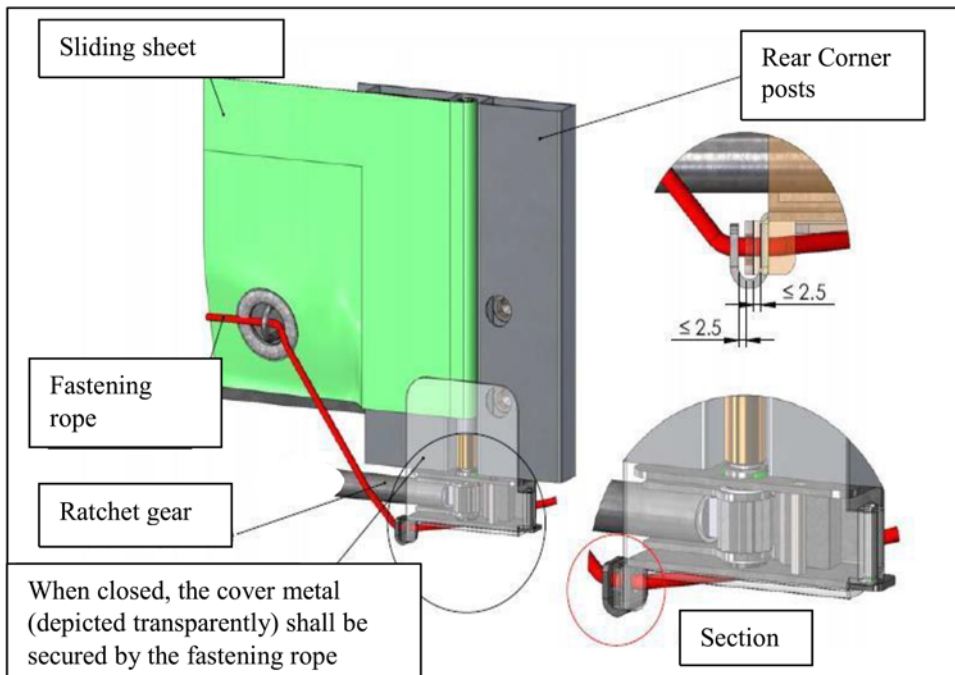
EXAMPLE OF A CONSTRUCTION OF A VEHICLE WITH SLIDING SHEETS



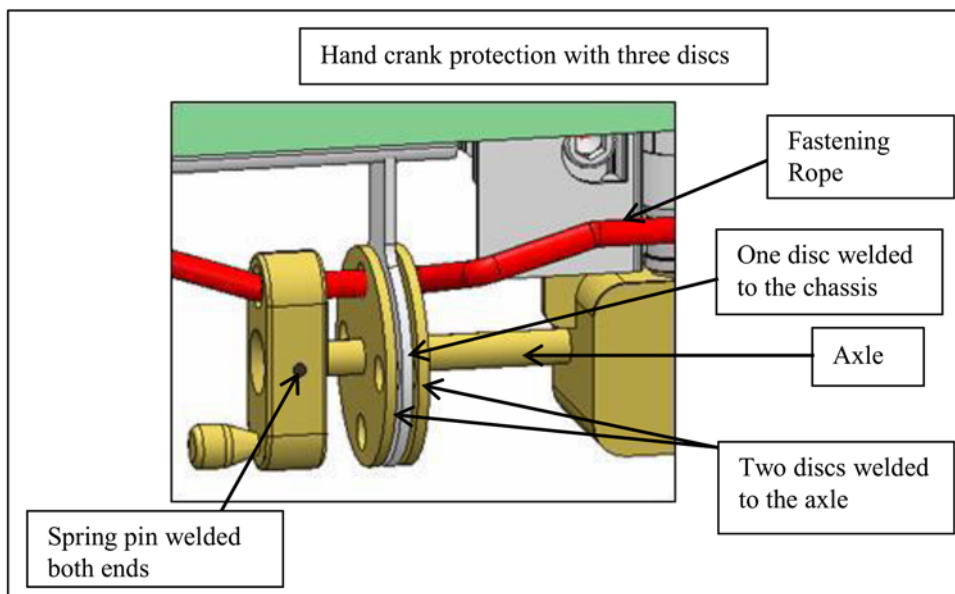
Sketch No. 9.4

To tighten the sliding sheets in the horizontal direction, a ratchet gear is used (normally at the rear end of the vehicle). This sketch shows two examples, (a) and (b), of how the ratchet or gearbox may be secured.

(a) Ratchet securing



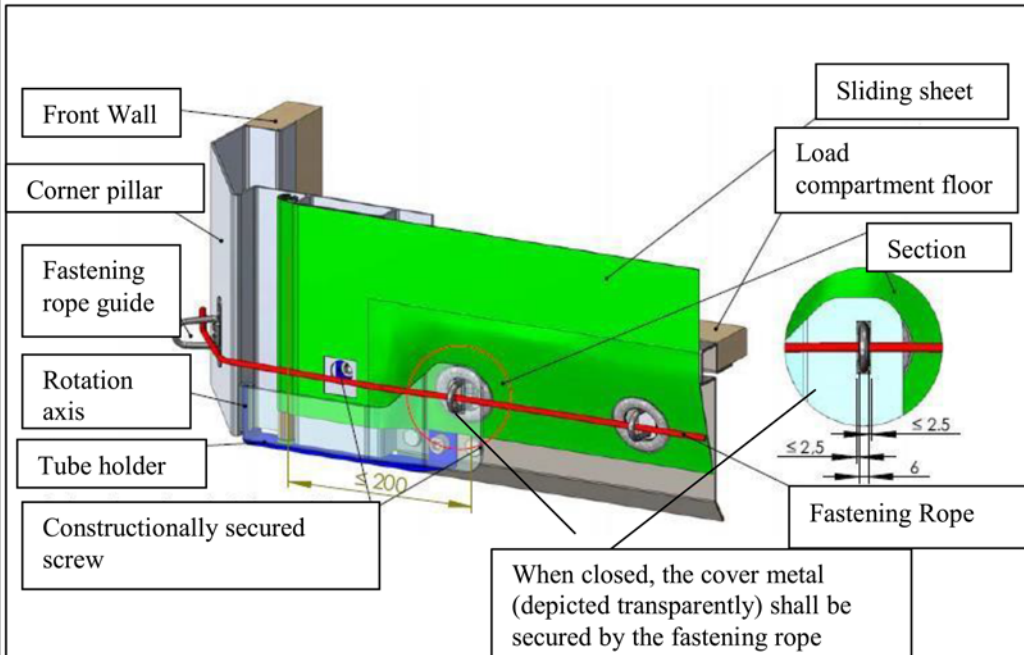
(b) Gearbox securing



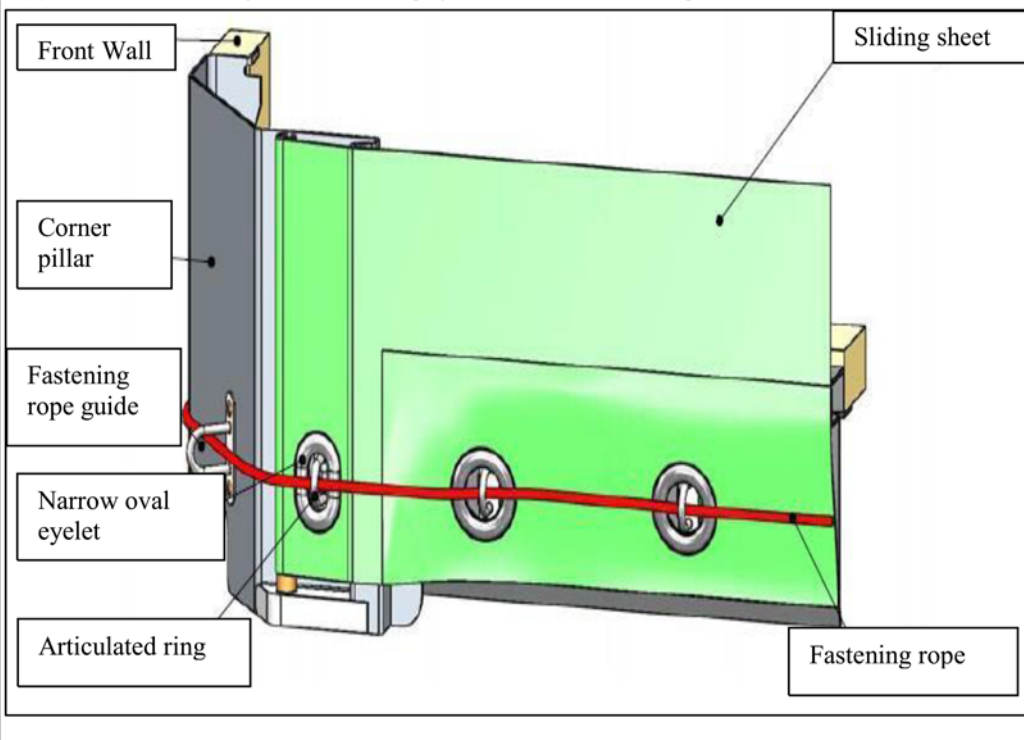
Sketch No. 9.5

To fix the sliding sheet on the other side (normally the front of the vehicle), the following systems, (a) or (b), may be used.

(a) Cover metal



(b) Narrow oval eyelet, anti-lifting system for the tensioning tube

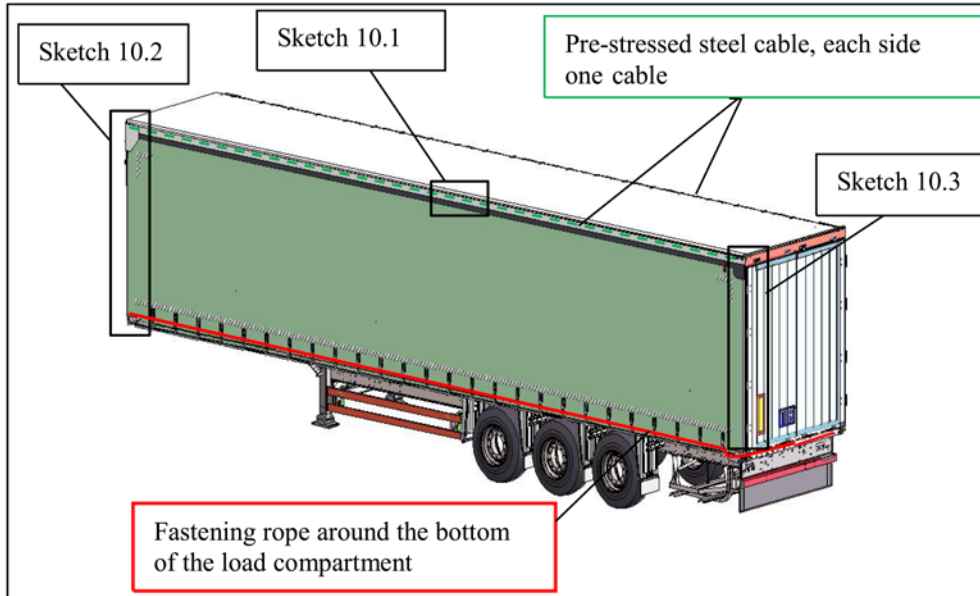


After new Sketch No. 9 insert

Sketch No. 10

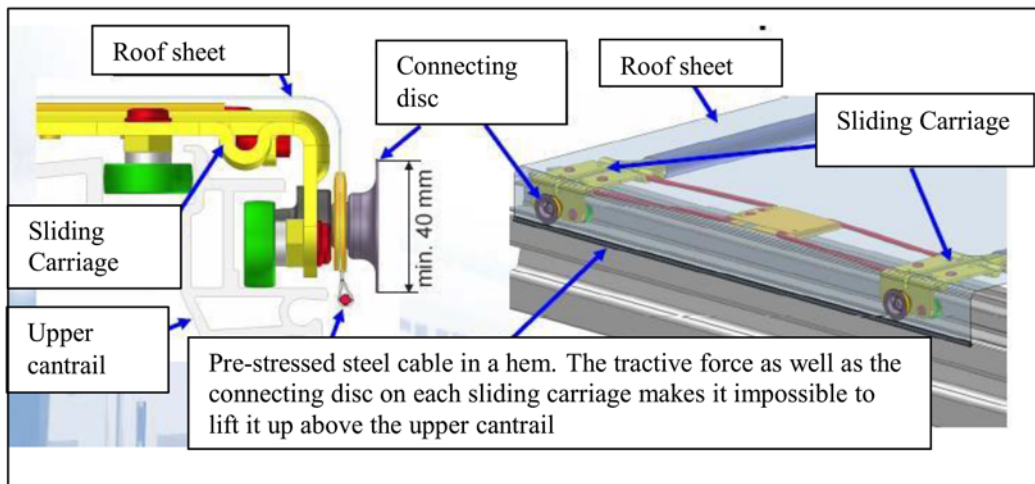
EXAMPLE OF A CONSTRUCTION OF A VEHICLE WITH A SHEETED SLIDING ROOF

This sketch shows an example of a vehicle and the important requirements described in Article 5 of these Regulations.



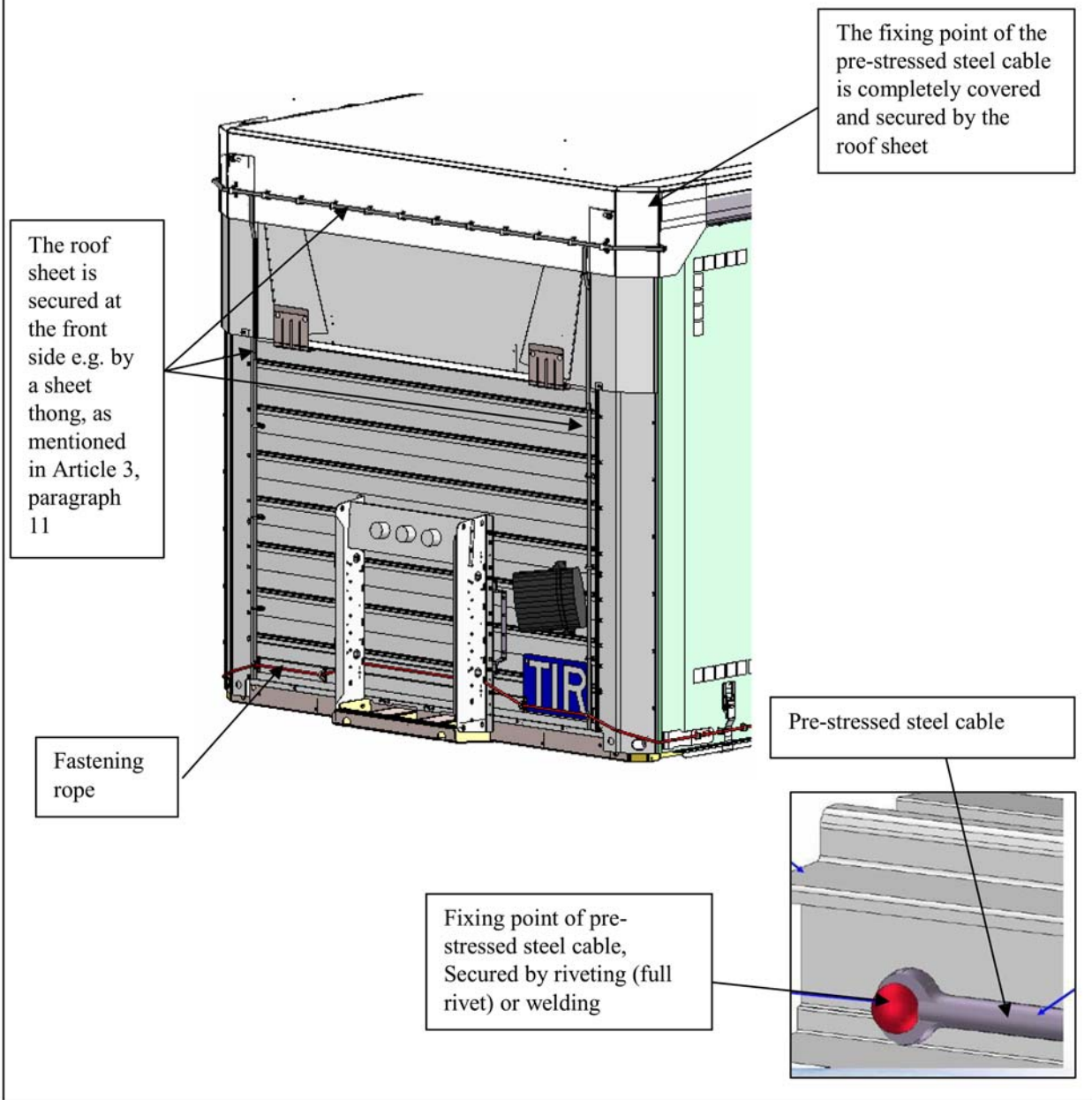
Sketch No. 10.1

Two pre-stressed steel cables, embedded in a hem, are fixed on each side of the load compartment. This pre-stressed steel cable is fixed to the front (see sketch 10.2) and rear of the body (see sketch 10.3). The tractive force as well as the connecting disc on each sliding carriage makes it impossible to lift up the hem with the pre-stressed steel cable above the upper cantrail.



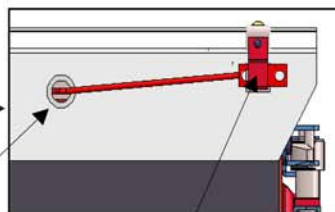
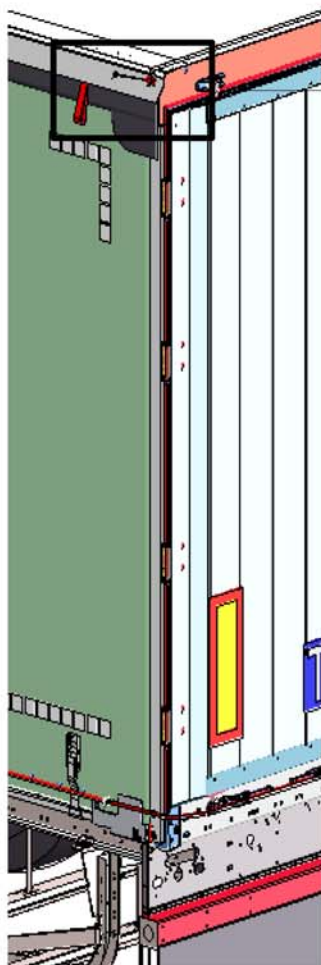
Sketch No.10.2

The sliding roof sheet shall overlap with the solid part of the roof at the front side of the load compartment, so that the roof sheet cannot be pulled over the top edge of the upper cantrail.



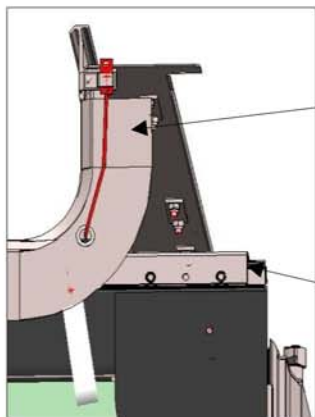
Sketch No.10.3

At the rear, a special device, such as a baffle plate, is fitted to the roof, preventing access to the load compartment, without leaving obvious traces when the doors are closed and sealed.



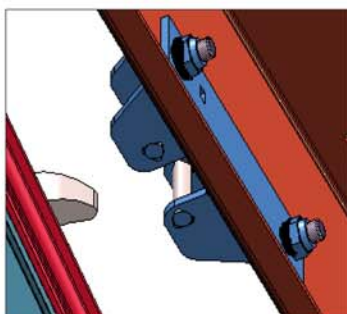
Pre-stressed cable goes in a hem

The fixing point of the pre-stressed steel cable is completely covered, and the metal cover is secured by welding or riveting (full rivet)



Tensioning device on the lever mechanism. By folding down the part of the roof with the tensioning device, the pre-stressed steel cable will be under tension

Sliding carriage from the roof sheet (closed) with lock system (inside)



By closing and sealing the doors, the systems are customs secure.

Annex 7, Part I, Article 5, paragraph 2, (i)

For the existing text substitute

- (i) The sliding sheets, floor, doors and all other constituent parts of the container shall be assembled either by means of devices which cannot be removed and replaced from the outside without leaving obvious traces, or by such methods as will produce a structure which cannot be modified without leaving obvious traces.

Annex 7, Part I, Article 5, paragraph 2, (iii)

For the existing text substitute

- (iii) The sliding sheet guidance, sliding sheet tension devices and other movable parts shall be assembled in such a way that when closed, and Customs sealed, doors and other movable parts cannot be opened or closed from the outside without leaving obvious traces. The sliding sheet guidance, sliding sheet tension devices and other movable parts shall be assembled in such a way that it is impossible to gain access to the container without leaving obvious traces once the closing devices has been secured. An example of such a system of construction is given in sketch No. 9 appended to these Regulations."

Annex 7, Part I, new Article 6

After the modified Article 5 insert

Article 6

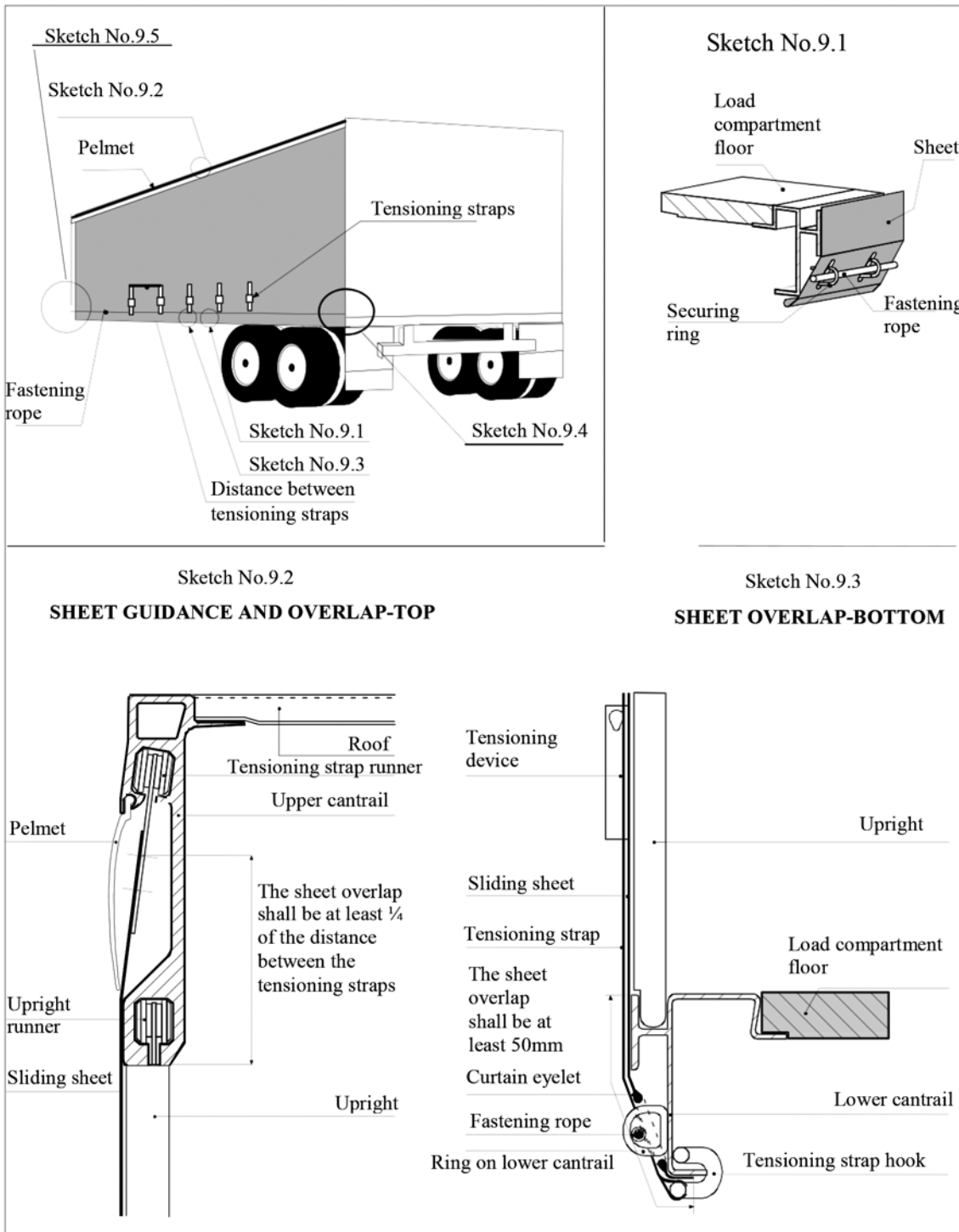
Containers with a sheeted sliding roof

1. Where applicable, the provisions of Articles 1, 2, 3, 4 and 5 of these Regulations shall apply to containers with a sheeted sliding roof. In addition, these containers shall conform to the provisions of this Article.
2. The sheeted sliding roof shall fulfil the requirements set out in (i) to (iii) below.
 - (i) The sheeted sliding roof shall be assembled either by means of devices which cannot be removed and replaced from the outside without leaving obvious traces, or by such methods as will produce a structure which cannot be modified without leaving obvious traces.
 - (ii) The sliding roof sheet shall overlap with the solid part of the roof at the front side of the container, so that the roof sheet cannot be pulled over the top edge of the upper cantrail. In the length of the container, at both sides, in the hem of the roof sheet, a pre-stressed steel cable shall be inserted in such a way that it cannot be removed and re-inserted without leaving obvious traces. The roof sheet shall be secured to the sliding carriage in such a way that it cannot be removed and re-secured without leaving obvious traces.
 - (iii) The sliding roof guidance, the sliding roof tension devices and other movable parts shall be assembled in such a way that when closed, and Customs sealed, doors, roof and other movable parts cannot be opened or closed from the outside without leaving obvious traces. The sliding roof guidance, sliding roof tension devices and other movable parts shall be assembled in such a way that it is impossible to gain access to the container without leaving obvious traces once the closing devices have been secured. An example of a possible system of construction is shown in sketch No. 10, appended to these Regulations.

For the existing Sketch No. 9 substitute

Sketch No. 9

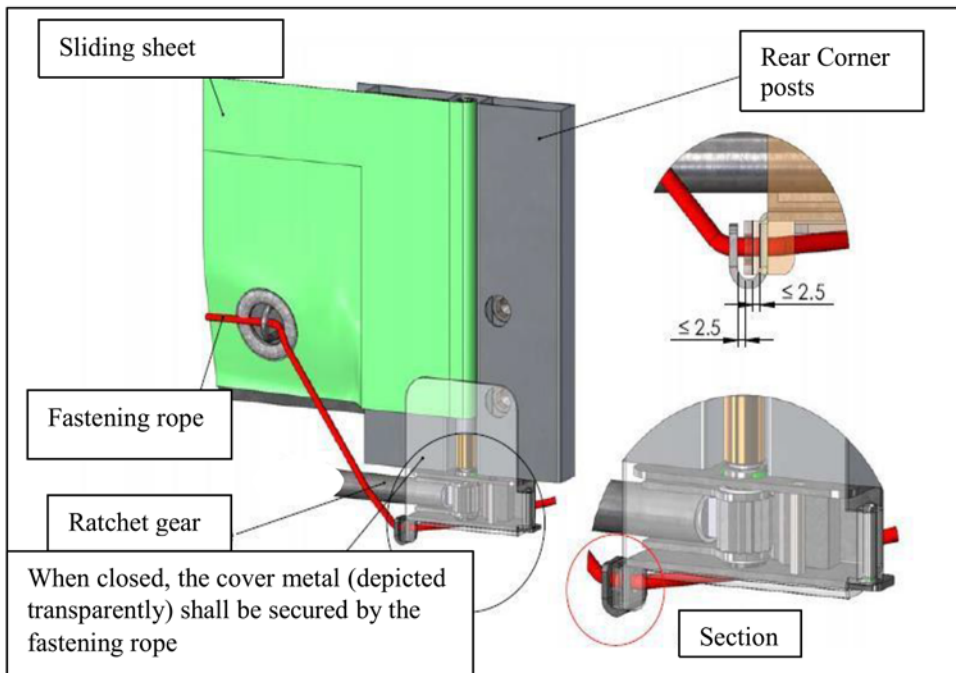
EXAMPLE OF A CONSTRUCTION OF A CONTAINER WITH SLIDING SHEETS



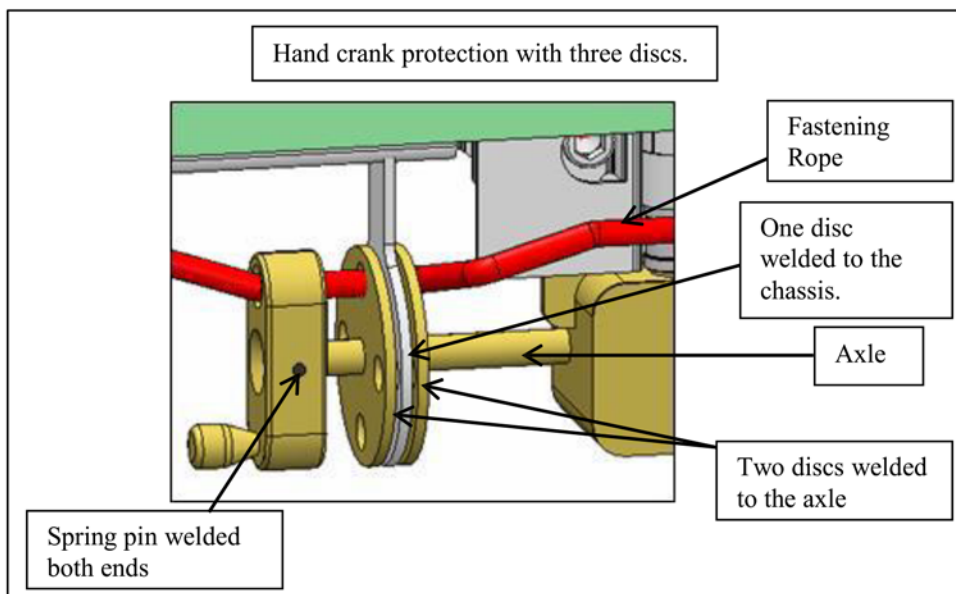
Sketch No. 9.4

To tighten the sliding sheets in the horizontal direction, a ratchet gear is used (normally at the rear end of the container). This sketch shows two examples, (a) and (b), of how the ratchet or gearbox may be secured.

(a) Ratchet securing



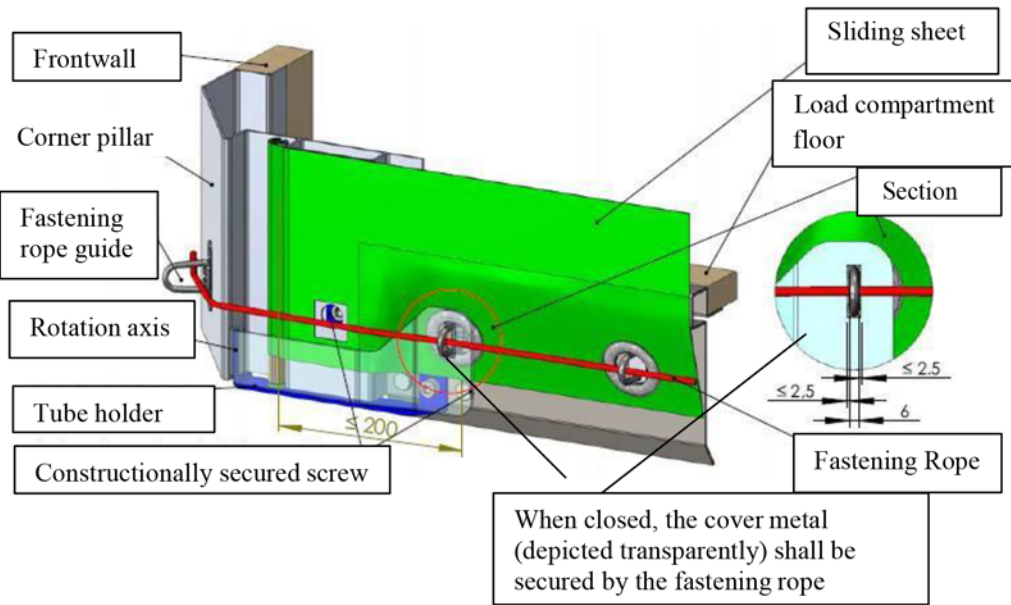
(b) Gearbox securing



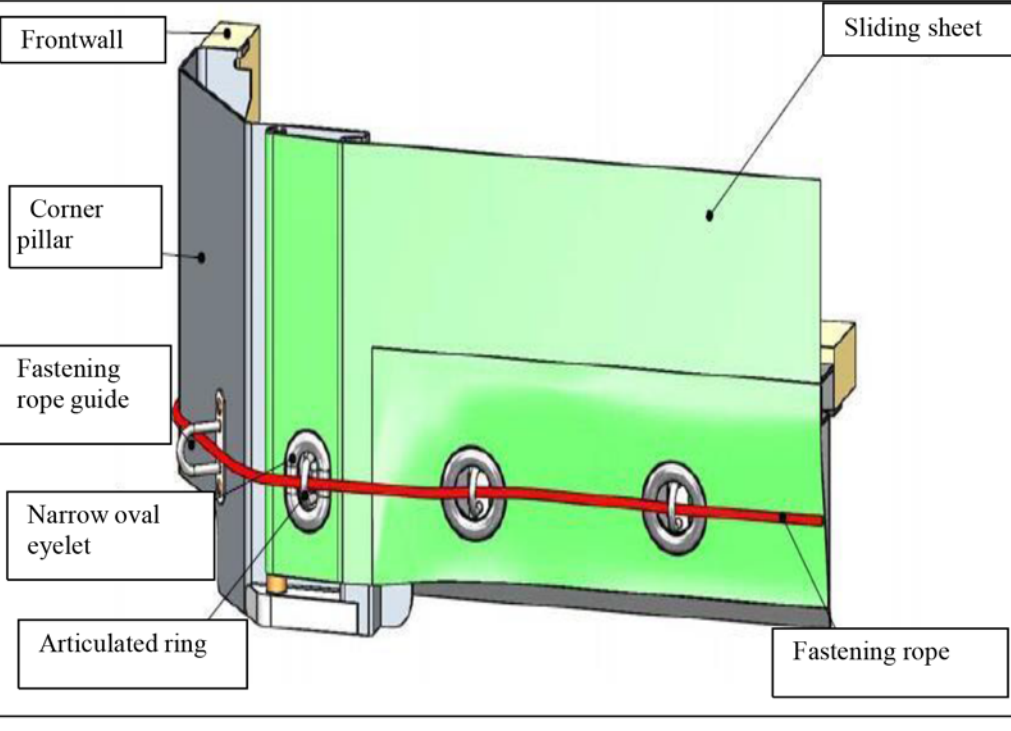
Sketch No. 9.5

To fix the sliding sheet on the other side (normally the front of the container), the following systems, (a) or (b), may be used.

(a) Cover metal



(b) Narrow oval eyelet, anti-lifting system for the tensioning tube

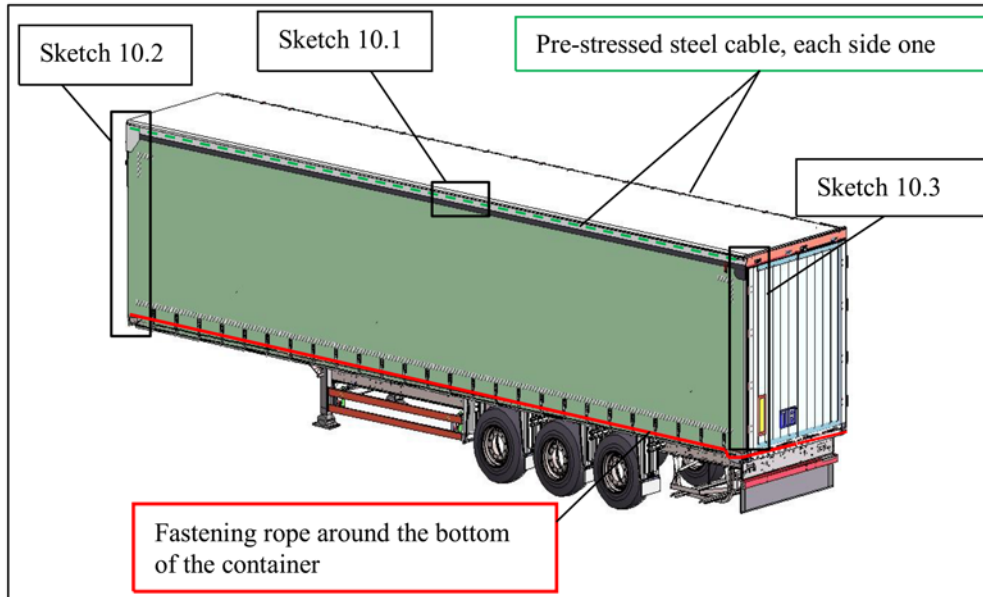


After new Sketch No. 9 insert

Sketch No. 10

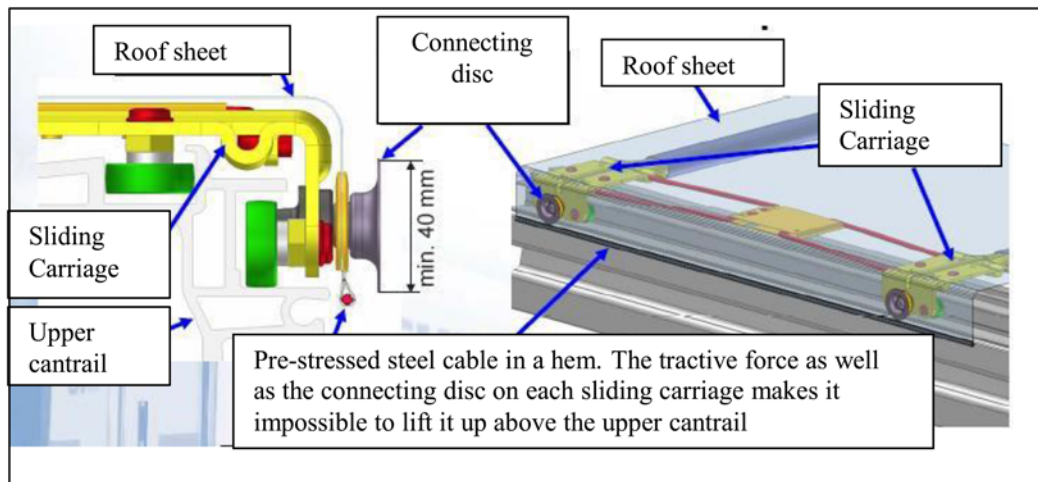
EXAMPLE OF A CONSTRUCTION OF A CONTAINER WITH A SHEETED SLIDING ROOF

This sketch shows an example of a container and the important requirements described in Article 6 of these Regulations.



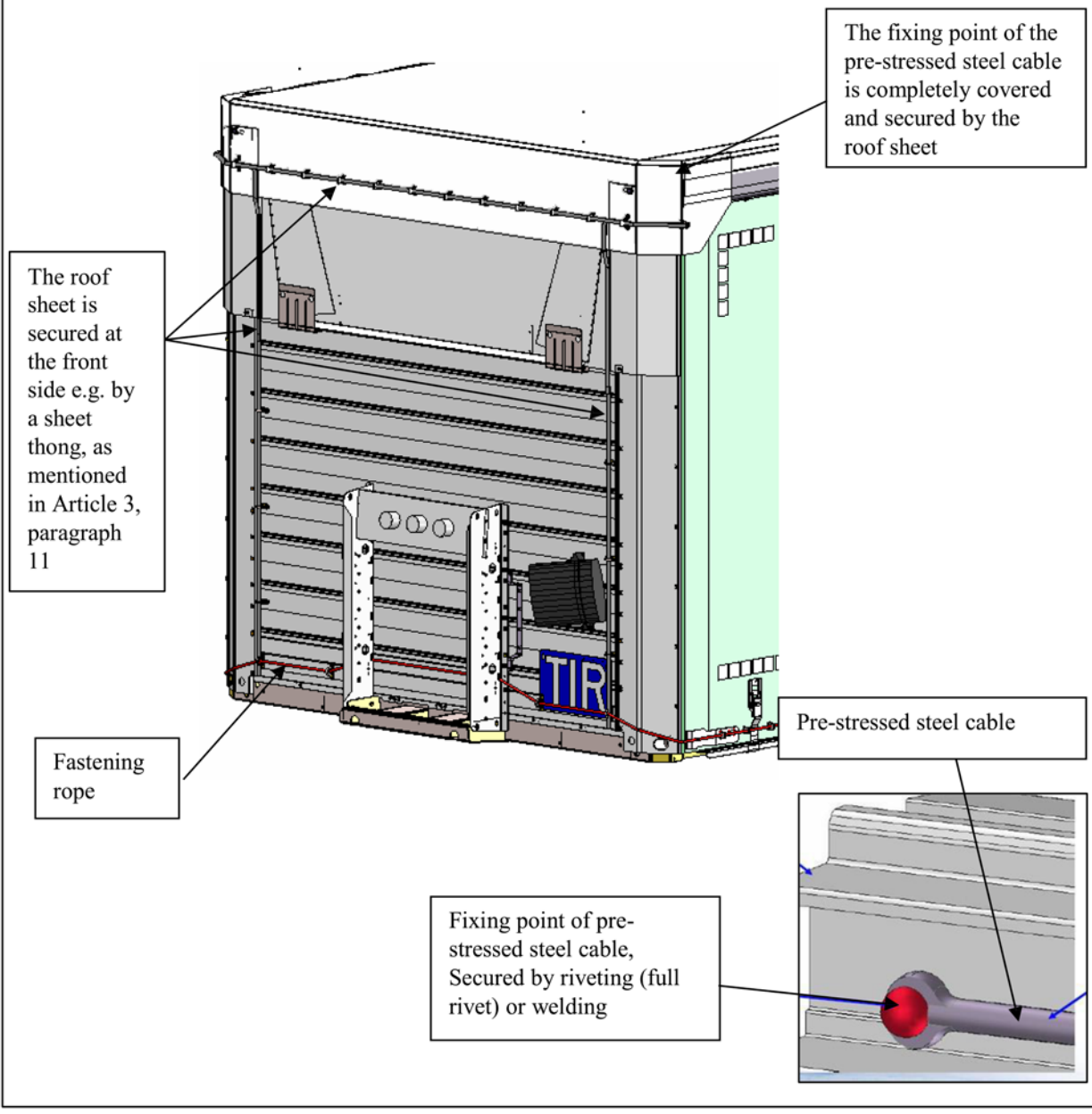
Sketch No. 10.1

Two pre-stressed steel cables, embedded in a hem, are fixed on each side of the container. This pre-stressed steel cable is fixed to the front (see sketch 10.2) and rear of the body (see sketch 10.3). The tractive force as well as the connecting disc on each sliding carriage makes it impossible to lift up the hem with the pre-stressed steel cable above the upper cantrail.



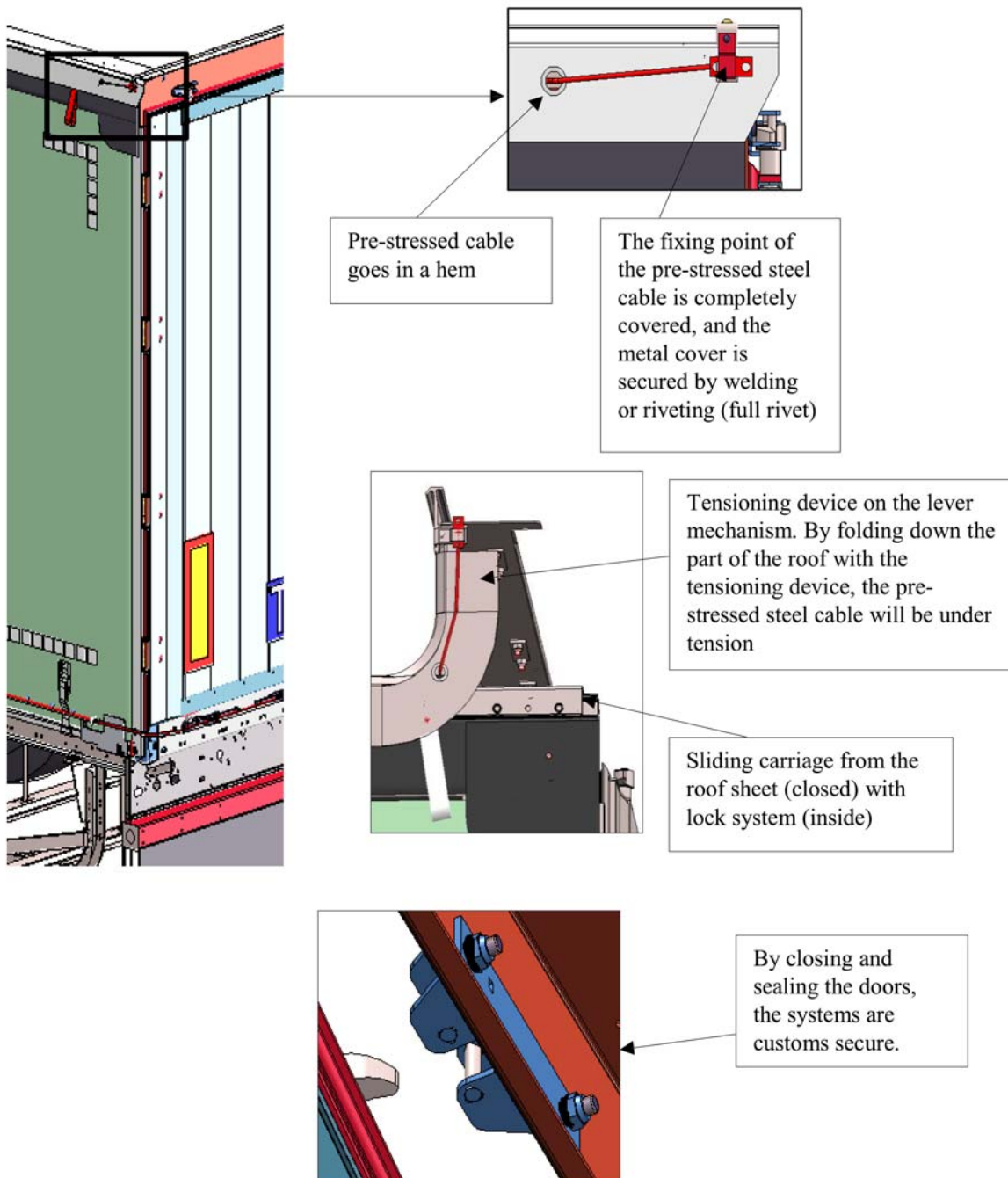
Sketch No.10.2

The sliding roof sheet shall overlap with the solid part of the roof at the front side of the container, so that the roof sheet cannot be pulled over the top edge of the upper cantrail



Sketch No.10.3

At the rear, a special device, such as a baffle plate, is fitted to the roof, preventing access to the container, without leaving obvious traces when the doors are closed and sealed.



D. PARLEMENT

Wijzigingen van 11 februari 2016

Voor Nederland (het Europese deel) behoeven de wijzigingen van de Bijlagen 2, 6 en 7 bij de Overeenkomst niet de goedkeuring van de Staten-Generaal, omdat zij een gebied (douanevervoer) bestrijken waarop de Europese Unie (EU) bij uitsluiting bevoegd is.

Voor Nederland (het Caribische deel), Aruba, Curaçao en Sint Maarten behoeven de wijzigingen van de Bijlagen 2, 6 en 7 bij de Overeenkomst ingevolge artikel 7, onderdeel f, van de Rijkswet goedkeuring en bekendmaking verdragen, evenmin de goedkeuring van de Staten-Generaal.

G. INWERKINGTREDING

Wijzigingen van 11 februari 2016

De wijzigingen van de Bijlagen 2, 6 en 7 bij de Overeenkomst zijn ingevolge artikel 59, derde lid, juncto artikel 60, eerste lid, van de Overeenkomst op 1 oktober 2016 aanvaard en op 1 januari 2017 voor alle verdragssluitende partijen, waaronder het Koninkrijk der Nederlanden, in werking getreden.

Wat betreft het Koninkrijk der Nederlanden, gelden de wijzigingen, evenals de Overeenkomst, voor het gehele Koninkrijk.

Koninkrijk der Nederlanden

Overeenkomst

Land	Voorlopige toepassing	In werking	Terugwerkende kracht	Buiten werking
Nederland (in Europa)		20-06-1983		
Nederland (Bonaire)		10-10-2010		
Nederland (Sint Eustatius)		10-10-2010		
Nederland (Saba)		10-10-2010		
Aruba		01-01-1986		
Curaçao		10-10-2010		
Sint Maarten		10-10-2010		

De Overeenkomst gold sinds 20-06-1983 voor de Nederlandse Antillen.

In overeenstemming met artikel 19, tweede lid, van de Rijkswet goedkeuring en bekendmaking verdragen heeft de Minister van Buitenlandse Zaken bepaald dat de wijzigingen van 11 februari 2016 van de Bijlagen 2, 6 en 7 bij de Overeenkomst zullen zijn bekendgemaakt in het gehele Koninkrijk op de dag na de datum van uitgifte van dit Tractatenblad.

Uitgegeven de *vierentwintigste* januari 2017.

De Minister van Buitenlandse Zaken,

A.G. KOENDERS